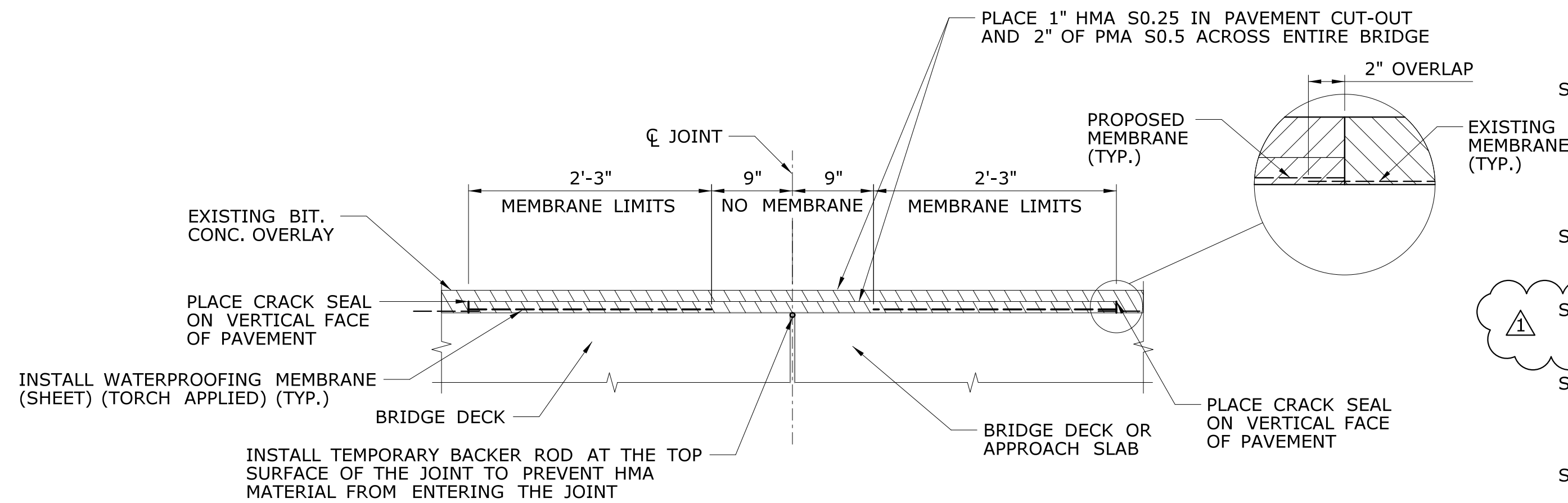
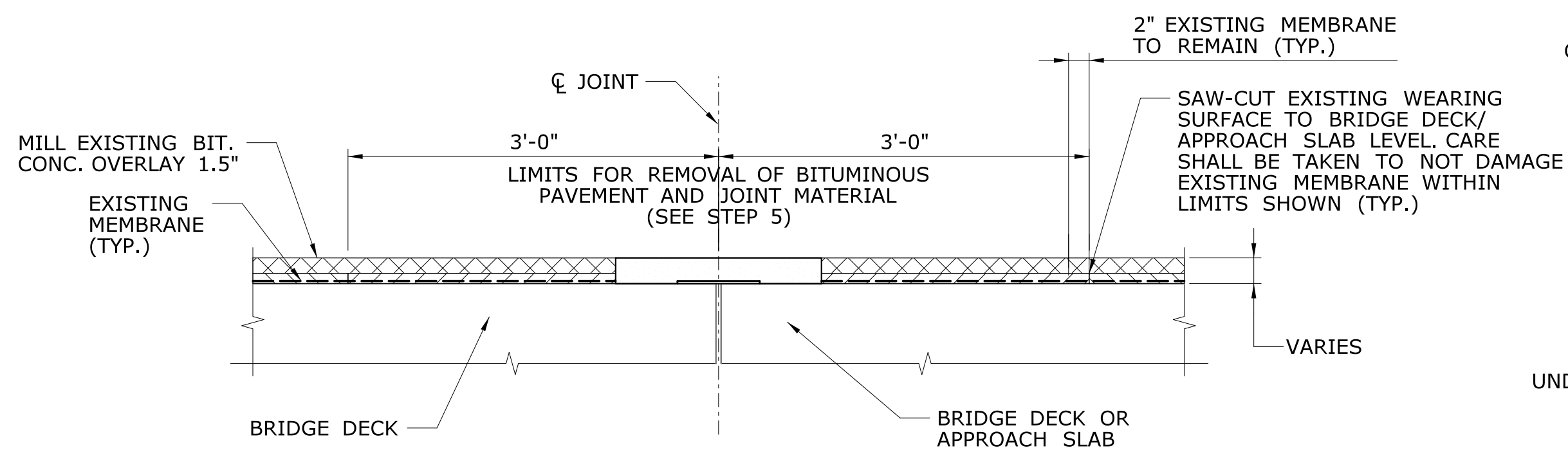


NOTES:
 1. REFER TO STEP 1 OF SEQUENCE OF WORK
 2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED BY THE CONTRACTOR.

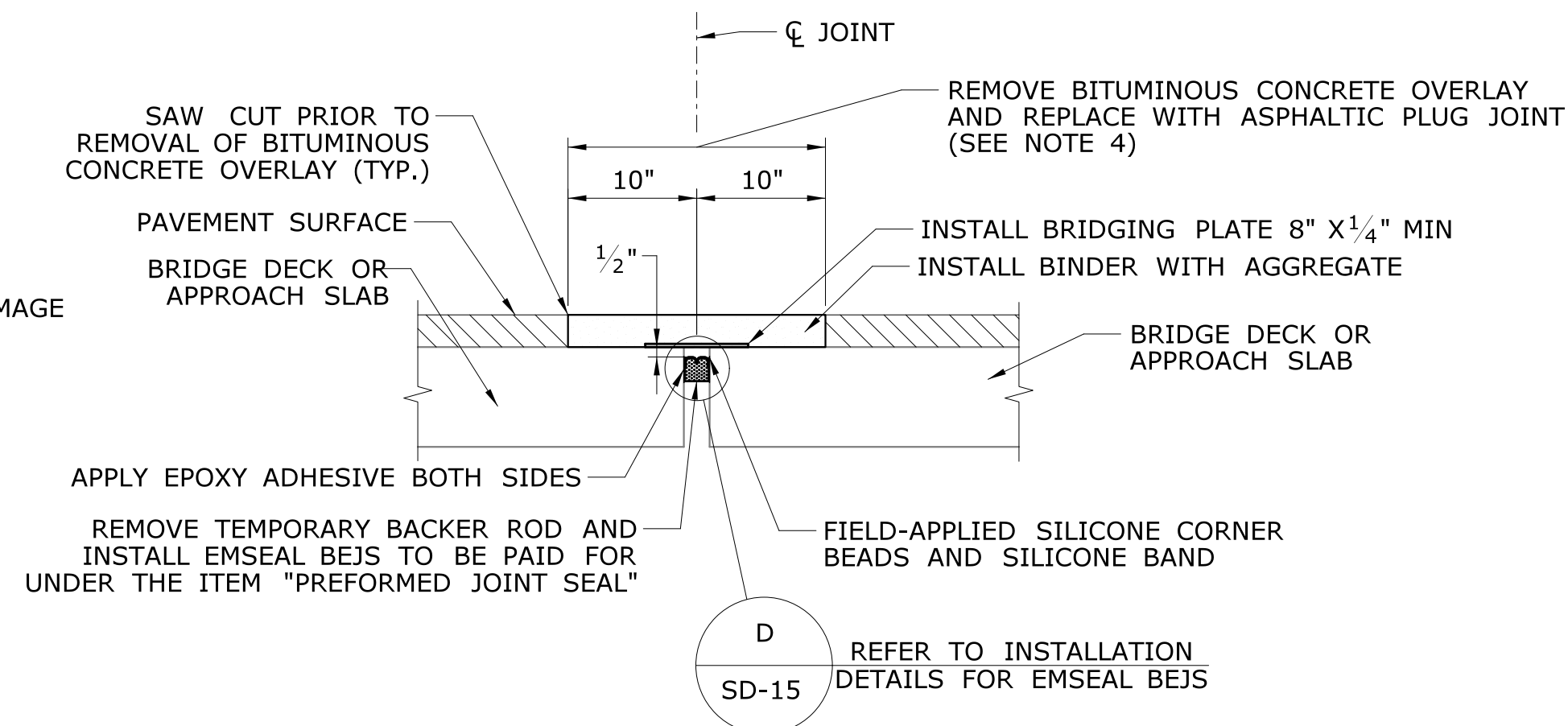
EXISTING CONDITION



**INTERMEDIATE CONDITION
 PLACEMENT OF PAVEMENT IN JOINT CUTOUT (STEPS 6-12)**



**INTERMEDIATE CONDITION
 JOINT AND PAVEMENT REMOVAL (STEPS 1-5)**



**FINAL CONDITION
 (STEPS 13-14)**

JOINT REPLACEMENT SEQUENCE
 SCALE: 1" = 1'-0"

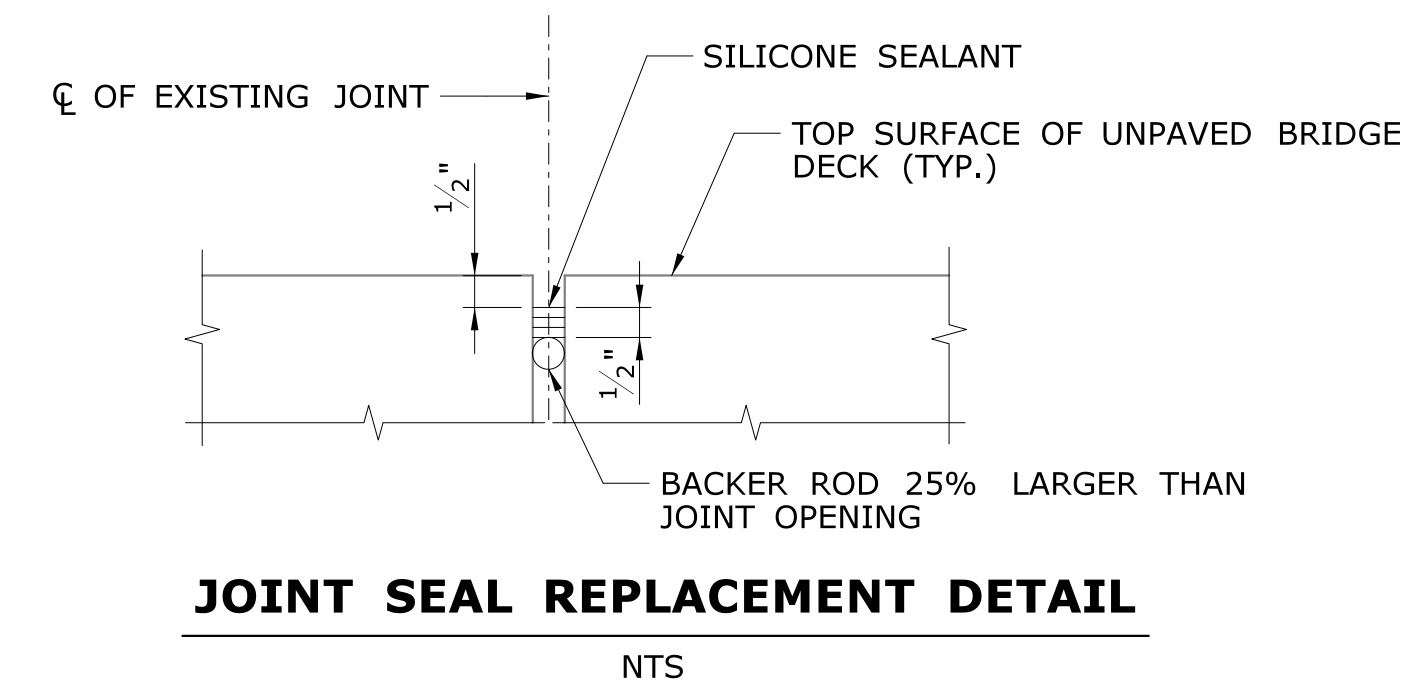
**ASPHALTIC PLUG EXPANSION JOINT SYSTEM
 AT ALL PIERS/ABUTMENTS EXCEPT PIERS 25 & 26**

SEQUENCE OF WORK

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2. CONTRACTOR SHALL ALSO MEASURE THE DECK JOINT GAP OPENING FOR SIZING OF EMSEAL BEJS. EMSEAL BEJS IS MADE TO ORDER. THE MANUFACTURER REQUIRES A MINIMUM OF THREE WEEKS LEAD TIME TO FABRICATE THE BRIDGE SEALS.
 - STEP 2: THE CONTRACTOR IS RESPONSIBLE FOR FILLING OUT THE EMSEAL BEJS CHECKLIST FOR EACH JOINT LOCATION. THE CHECKLIST SHALL BE FORWARDED TO EMSEAL AND BRIDGE DESIGN FOR REVIEW.
 - STEP 3: MILL 1.5" OF EXISTING BITUMINOUS OVERLAY WITHIN BRIDGE LIMITS DETERMINED BY THE NUMBER OF JOINTS WHICH CAN BE REPLACED WITHIN A 14-DAY PERIOD.
 - STEP 4: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
 - STEP 5: REMOVE EXISTING PAVEMENT MATERIAL, MEMBRANE (IF PRESENT), AND JOINT MATERIAL INCLUDING BACKING PLATE WITHIN THE LIMITS SHOWN.
 - STEP 6: INSTALL TEMPORARY BACKER ROD FLUSH WITH THE BRIDGE DECK AND APPROACH SLAB.
 - STEP 7: REPAIR DETERIORATED TOP SURFACE OF CONCRETE DECK AS REQUIRED.
 - STEP 8 (OPTIONAL): INSTALL TEMPORARY PAVEMENT WITH BOND BREAKER IF REMAINING WORK CANNOT BE COMPLETED BEFORE OPENING HIGHWAY TO TRAFFIC. TEMPORARY PAVEMENT SHALL BE REMOVED PRIOR TO PROCEEDING TO STEP 9.
 - STEP 9: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN.
 - STEP 10: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
 - STEP 11: PLACE 1" HMA S0.25 TRAFFIC LEVEL 2 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
 - STEP 12: PLACE PMA S0.5 OVER THE MILLED PORTION OF THE BRIDGE
 - STEP 13: CUT PAVEMENT FULL DEPTH, 10" EACH SIDE OF CENTER OF JOINT, AND REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS.
 - STEP 14: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM WITH EMSEAL BEJS AND BRIDGING PLATE. LOCATING PINS SHALL NOT BE USED TO SECURE THE BRIDGING PLATE.
- (REPEAT SEQUENCE UNTIL ALL JOINTS ARE REPLACED)

ASPHALTIC PLUG EXPANSION JOINT NOTES:

1. REMOVAL OF BITUMINOUS CONCRETE OVERLAY AND REPLACEMENT WITH ASPHALTIC PLUG EXPANSION JOINT SYSTEM TO BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM". (SEE SPECIAL PROVISION)
2. REPAIR CONCRETE BRIDGE DECK AS NEEDED TO BE PAID FOR UNDER "FULL DEPTH PATCH (HIGH EARLY STRENGTH CONCRETE)" OR "PARTIAL DEPTH PATCH."
3. SAW-CUTS MADE 3' EACH SIDE OF CENTERLINE OF JOINT WILL BE PAID AS "CUT BITUMINOUS CONCRETE PAVEMENT".
4. SAW-CUTS MADE 10" EACH SIDE OF CENTERLINE OF JOINT WILL BE INCLUDED IN THE COST OF "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
5. THE REMOVAL OF ALL EXISTING JOINT SYSTEMS AND BITUMINOUS CONCRETE WITHIN THE LIMITS SHOWN TO BE INCLUDED FOR PAYMENT UNDER THE ITEM, "REMOVAL OF EXISTING WEARING SURFACE".
6. INSTALLATION OF MEMBRANE WITHIN THE LIMITS SHOWN TO BE PAID UNDER THE ITEM, "MEMBRANE WATERPROOFING (SHEET) (TORCH-APPLIED)".
7. CRACK SEALANT PLACED ALONG VERTICAL FACES OF THE SAW-CUT PAVEMENT TO BE PAID UNDER THE ITEM, "JOINT AND CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT".
8. TEMPORARY CLOSED CELL BACKER ROD DIAMETER SHALL BE DETERMINED AFTER MEASURING THE JOINT OPENING. THE ROD SHALL BE 25% LARGER THAN THE JOINT OPENING.
9. THE FURNISHING AND PLACING OF HMA S0.25 AND PMA S0.5 TO BE INCLUDED FOR PAYMENT UNDER THE ITEMS "HMA S0.25" AND "PMA S0.5" RESPECTIVELY.
10. EMSEAL BEJS PAID UNDER "PREFORMED JOINT SEAL."



- NOTES:
 1. THIS WORK SHALL INCLUDE THE REMOVAL OF THE EXISTING JOINT MATERIAL AND SHALL BE PAID FOR UNDER "REPLACE JOINT SEAL", SEE SPECIAL PROVISION.
 2. SILICONE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS BETWEEN 50° F AND 80° F.

ADDENDUM NO. 3

DESIGNED BY:
 ALFRED BENESCH & CO.

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
1	11/16/17	REVISED SEQUENCE OF WORK NOTES	07.14.A3

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: \$DATES

DESIGNER/DRAFTER:
M. HABEK

CHECKED BY:
S. DRECHSLER

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION

SCALE AS NOTED

SIGNATURE/BLOCK:
 Alfred Benesch & Company
 90 National Drive
 Glastonbury, CT

PROJECT TITLE:
**REHABILITATION OF BRIDGE NOS.
 03367 & 03368**

TOWN:
HARTFORD

DRAWING TITLE:
DECK JOINT DETAILS - 1

PROJECT NO.
063-705

DRAWING NO.
SD-14

SHEET NO.
07.14.A3